

### **11-6-113 Off-Street Parking.**

(1) The purpose for off-street parking requirements is to provide adequate parking for the intended use of a parcel while eliminating the need to park in the public right-of-way. Every building, structure, improvement, and use shall provide permanent, maintained off-street parking as specified in this Chapter. The parking shall be a continuing obligation of the property owner as long as the use continues. It shall be unlawful for a land owner to eliminate required off-street parking unless otherwise provided on the parcel and approved by the City.

(a) Parking for Non-Conforming Uses: Any use of property which, on the effective date of this Chapter or of any subsequent amendment, is nonconforming only with the regulations relating to off-street parking facilities may continue in the same manner, provided that parking facilities shall not be further reduced.

(b) Parking for Mobile Home Parks: This ordinance shall not apply to the RMHP Zone. Regulations for off-street parking in that zone are contained in the development and design standards for mobile home parks.

(2) General Provisions: The following general provisions apply to all off-street parking requirements of this Chapter.

(a) Location: The location of off-street parking facilities shall be as follows, and shall not be located within the public right-of-way:

(i) For single family, duplex, and multi-family residential dwellings, required parking shall be located on the same lot as the building which they are required to serve. Required off-street parking shall not be located in a required front yard or side yard that abuts a street setback. (amended by Ord. No. 36-2006, 12/05/2006)

(ii) Parking for multiple-family buildings shall be located either behind the building or 20' behind the front setback of a building within an interior side yard. The Commission may approve front loaded garages in multi-family buildings when the designated parking is at least 20' behind the front façade and the width of the garage façade facing the street is less than 34% of the total building façade facing the street. (Ord. No. 20-05, 10/04/2005)

(iii) Parking in the VC-Village Center and TC-Town Center Districts shall be located behind the building.

(iv) For all other uses, including commercial and industrial uses, required parking shall be located on the same lot or on an abutting lot. In no case shall required parking be located across a public street without written approval of the Planning Commission and only when the parking area is within 200 feet.

(v) If parking is located on a lot or parcel under different ownership, a perpetual easement must be recorded in the office of the Utah County Recorder prior to final approval.

(vi) Tandem parking (one space in front of another) will not be allowed to satisfy the requirements found herein.  
(Amended by Ord. No. 26-2006, 12/05/2006)

(b) Lighting: Exterior lighting shall be appropriate to the task. Any lights provided or required to illuminate a parking area shall be arranged in a manner that will reflect light away from adjacent properties.

(c) Shared Parking: The Planning Commission may approve the joint use of a parking lot or facility provided that the applicant can show that conflict between the different users can be effectively eliminated. For instance, willing parties such as a church and a commercial business may share parking facilities if typical heavy use parking times allow. Copy of recorded perpetual easement shall be provided to the Community Development Director.

(d) Construction plans for any parking facility or the alteration of any parking facility shall be submitted to the City for review and shall be approved by the Planning Commission. A parking lot or structure may be approved as part of the project approval.

(e) Change of use: Whenever the use of any building is changed, the new use shall not be permitted and a business license shall not be issued until and unless the premises comply with the requirements contained herein.

(3) Design and Construction Standards: All parking facilities shall satisfy the following minimum standards for design; of parking stalls and construction standards:

(a) All parking facilities shall be hard surfaced with asphalt or concrete or similar impervious materials. The materials shall meet the approved standards and specifications and be capable of handling the anticipated size and weight of vehicles, including public safety vehicles.

(b) Each parking lot shall be surrounded by a concrete curb, or other border approved by the City Engineer to ensure the life of the surface and to limit the access to approved ingress and egress.

(c) All parking spaces shall have minimum dimensions of nine (9) feet by twenty (20) feet for parking at right angles and shall require twenty-four (24) feet backing distance in aisle width. Parallel parking spaces shall have a minimum length of twenty-two (22) feet.

(d) Requirements for parking developed at varying angles are according to Table 1 below and included in this Section. Parking stalls designed at less than a 90

degree angle shall be allowed for one-way traffic only.

(e) Parking shall be designed to avoid backing onto a public street. Except on minor local streets where the size or shape of the parcel is such that development would be precluded otherwise.

(f) Parking for the handicapped shall be provided at a size, number, location, and with signage as specified by state and federal regulations.

(g) Parking in all R1 zones shall have two spaces covered; a minimum of one space must be covered for single-family dwellings in all other residential zones; and two of the four spaces required for duplexes must be covered in their respective zones.

(h) In addition to any other landscaping requirements listed in Section 11-6-208-210, parking lots with 200 spaces or more shall include a landscaped area, which includes a walkway connecting the street right-of-way to the store entrance of at least ten (10) feet in width.

(i) Separation from buildings – Excluding single family and two family dwellings, all off-street parking spaces and interior access drives shall maintain a minimum five (5) foot separation from the wall of a building

TABLE 1. PARKING ANGLE SPACE DIMENSIONS

	A	B	C
Parking Angle	Stall Width	Stall Length	Aisle Width
45°	9'	18'	13'
60°	9'	19'	17'
90°	9'	20'	24'
<u>Parallel</u>	<u>9'</u>	<u>22'</u>	

FIGURE 1.

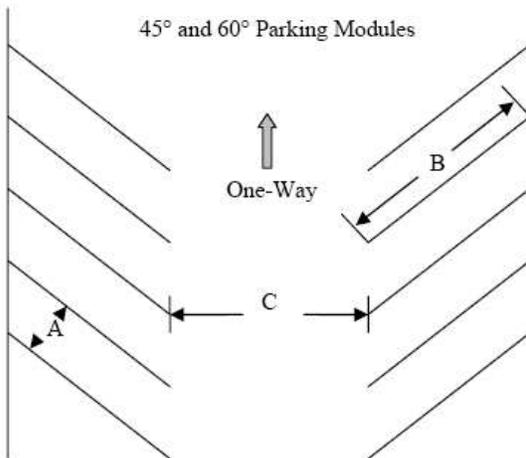
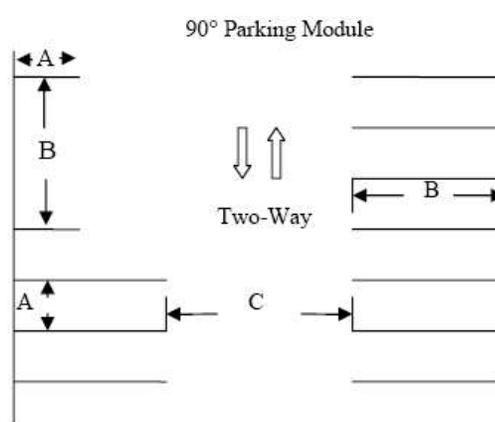


FIGURE 2.



The letters in the Figures 1 and 2 correspond to the dimensions in Table 1 above.

(4) Maintenance: All parking facilities shall be maintained and kept free of garbage and debris. Striping of parking stalls shall be kept in a manner that allows each stall to be identified. Potholes, cracks, and other damage to the surface shall be repaired in a timely manner.

Any violation of this Section shall be determined to be a Zoning Violation and shall be punishable by a Class B misdemeanor pursuant to Section 12-2-21 of Springville City Code.

(5) Reduction in Parking Standards:

(a) An applicant may apply to the Planning Commission for a reduction in the amount of parking spaces. The applicant must clearly demonstrate that the required number of parking stalls is unnecessary for the proposed use and any possible future use of the building. Any request which lowers the amount of parking stalls by more than twenty (20) percent shall not be approved.

(b) For the Historic Downtown Area (which is defined as that area zoned as TC Town Center and generally located between Center Street and 400 South from 100 East to 100 West) the Planning Commission may grant a parking reduction based upon the following considerations:

(i) A reduction in parking will still adequately meet the unique needs of the proposed land use;

(ii) A reduction in parking will not cause a shortage for other uses located in the area; and

(iii) A reduction in parking will contribute to the vitality and overall well being of the Historic Downtown Area.

(6) Parking Spaces Required: Off-street parking shall be provided as follows for all new buildings and structures, or enlargement of any existing buildings or structures. Should the required parking stalls as calculated using the following standards end in a fraction, the standard shall be rounded up to the next whole number.

(a) Requirements for types of buildings and uses not specifically listed herein shall be determined by the Planning Commission, based upon the requirements for the most comparable uses specified herein. Buildings with more than one use shall provide parking required for each use.

<b>USE</b>	<b>MINIMUM NUMBER OF PARKING SPACES REQUIRED</b>
Single-Family Residential, (detached or attached)	Two (2) spaces per unit
Multi-Family Residential	Two (2) spaces per unit plus .25 spaces per unit for visitor parking.
Elderly / Assisted Living Facility	One (1) space per every two (2) beds + one (1) space per employee.
Business Office	One (1) space per 300 square feet of floor area.
Medical/ Professional Office/Clinic	One (1) space per 150 square feet of floor area.
Commercial / Retail	One (1) space per 300 square feet of floor area.

<b>USE</b>	<b>MINIMUM NUMBER OF PARKING SPACES REQUIRED</b>
Restaurant	One (1) space per four (4) seats or one (1) space per 100 square feet of gross floor area, including outside seating, whichever is greater.
Health / Recreation / Amusement	One (1) space per 150 square feet of floor area.
Automotive Service / Repair	Five (5) spaces per service bay.
Hotels / Motels	One (1) space per room + two (2)
Churches	One (1) space per 100 square feet of floor area.
Schools / Elementary	Two (2) spaces per classroom.
Schools / Secondary	One (1) space per employee + One (1) space per Four (4) students.
Personal Services / Barbers / Beauty / Travel	One (1) space per 200 square feet of floor area.
Public Service or Utility	One (1) space per employee on the largest work shift, + one space per company vehicle normally stored or parked on the premises, + one (1) space per five hundred (500) square feet of floor area.
Self-Storage Facilities	Three (3) spaces per 1000 square feet of floor area devoted to office space, plus one (1) space per resident / manager, plus one (1) space for each fifty (50) storage units. Required parking spaces shall not be utilized as rental or leased spaces.
Theater – Indoor	One (1) space for every three (3) seats.
Manufacturing	One (1) space per employee on the largest work shift.

(b) Floor areas used in calculating the required number of parking spaces shall be gross floor areas of the building, calculated from the exterior outside wall without regard to specific inside uses. In mixed-use facilities, calculations shall be based on gross square footage of each identifiable use within the building and the total square footage of each identifiable use shall be the same as the gross floor area calculated from outside wall to outside wall.

(7) Stacking Space for Drive-In Businesses: All plans shall show the location, size and dimensions of all such facilities. The plans shall follow the stacking space schedule and shall demonstrate that such facilities will not result in the stacking of vehicles on public rights of way.

(a) Drive-in facilities shall be located in side or rear locations that do not interrupt direct pedestrian access along connecting pedestrian frontage.

(b) The length of stacking spaces shall be twenty (20) feet.

Use	Minimum Number of Stacking Spaces	Measured From
Bank teller lane	4	Teller or window
Automated teller machine	3	Teller
Restaurant drive-in	8	Pick up window
Car wash stall – automatic	6	Entrance
– manual	2	Entrance
Attended parking lot or structure	3	Attendant
Gasoline pump	2	Each end of pump island
Other		Determined by the City of Springville’s Community Development Director

(8) Bicycle Parking: Every parking facility which is required to provide at least forty (40) vehicular parking spaces shall be required to provide bicycle parking spaces at a rate of one (1) bicycle parking space per every twenty (20) vehicular parking spaces.

(a) Bicycle parking spaces shall have a minimum width of two (2) feet and a minimum length of six (6) feet, unless the spaces are provided by a premanufactured bicycle rack or locker, which differ from this dimension, in which case the dimension of the pre-manufactured rack or locker shall suffice.

(b) Required bicycle parking facilities shall, at a minimum, provide a stationary object to which the operator can lock the bicycle frame and both wheels with a user provided U-shaped lock or cable and lock.