

## 15-06-02 General Non-Residential Development Standards

### D. Driveway Access.

#### 1. General Standards.

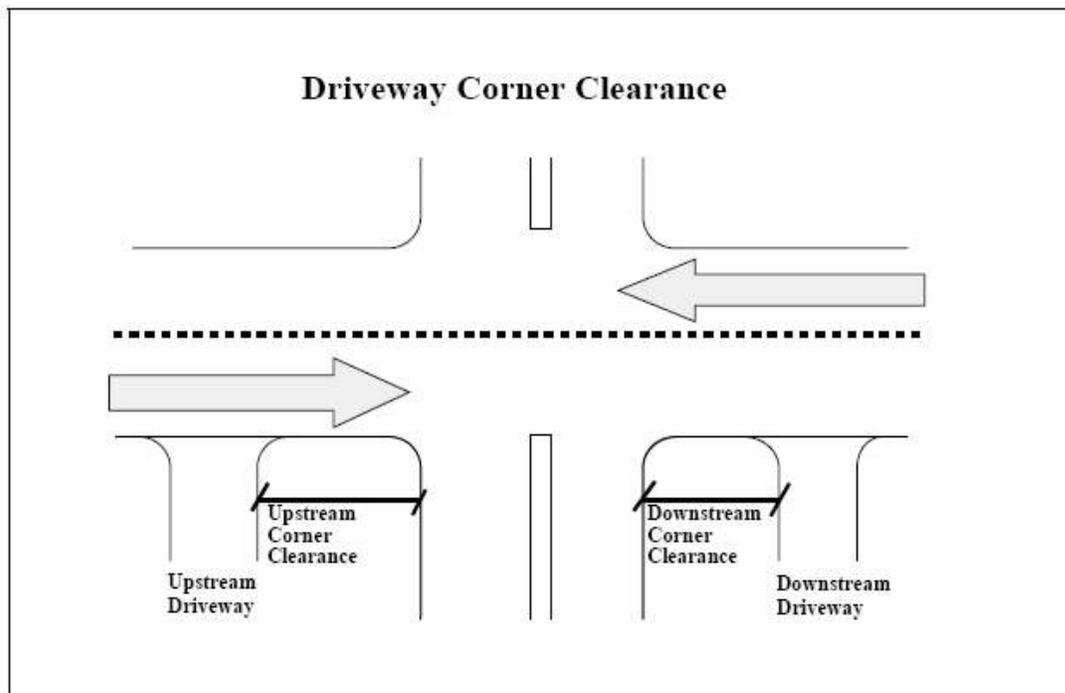
- a. Unobstructed and direct driveways shall be provided from commercial off-street parking or loading facilities to a street or alley. Loading driveways may coincide with driveways to parking facilities.
- b. In establishing permissible curb openings and sidewalk driveway crossings for access to private property, such curb openings or driveways shall not be authorized where they are unnecessary or where they would interfere with the movement of vehicular traffic, with public improvements, or with the rights of the public in the adjacent street or alley. In no case shall any curb opening be of greater width than necessary for reasonable access to the property to be served.
- c. In determining the width of curb openings and spacing of driveways, the end transitions in each case will not be considered a part of the length of the curb opening. The curb opening or width of each driveway shall be defined as the throat width of the driveway at the inside point of the curb transition radius at the top face of the curb.
- d. Only *one driveway opening per street frontage/per parcel* shall be allowed unless a capacity or safety need for more than one driveway opening can be demonstrated to the Sandy City Transportation Engineer.
- e. Where commercial uses share a property line, off-street parking lots serving the properties shall be made accessible to each other unless grade differences or building locations make reciprocal access between developments impractical.

#### 2. Driveway Separations.

- a. **Frontage on Arterial (106'+) or Major Collector Streets (80').** Uses on parcels with less than 150 feet of frontage shall be required to share a common driveway in order to assure that a minimum of 170 feet of continuous curb and gutter exists from the throat of one driveway to the throat of the next adjacent driveway. Driveways offset less than one hundred seventy [170] feet from existing or approved driveways on the opposite side of the street shall not be allowed unless the Sandy City Transportation Engineer determines that an unacceptable capacity or safety impact will not result.
- b. **Frontage on Minor Collector (60' or 66') or Local Streets (50').** Uses with less than 70 feet of frontage shall be required to share a common driveway in order to assure that a minimum of 90 feet of continuous curb and gutter exists from the throat of one driveway to throat of the next adjacent driveway. Driveways offset less than one hundred seventy [170] feet from existing or approved driveways on the opposite side of the street shall not be allowed unless the Sandy City Transportation Engineer determines that an unacceptable capacity or safety impact will not result.
- c. **Driveways Adjacent to Intersections.** The minimum distance from the intersection to the nearest driveway shall be according to the following intersection illustration and distance table (distances are measured from the back of curb to the throat of the nearest edge of the driveway).

- d. **Deviations to Driveway Separation.** The Planning Commission shall review and may approve or deny deviations to the above standards upon the recommendation of the Sandy City Transportation Engineer.

	Median Barrier Present	Arterial	Major Collector	Minor Collector
Driveway Clearance	NO	200'	175'	50'
Driveway Clearance	YES	185'	115'	50'



3. Driveways - Widths and Curb Designs.

a. **One Way.**

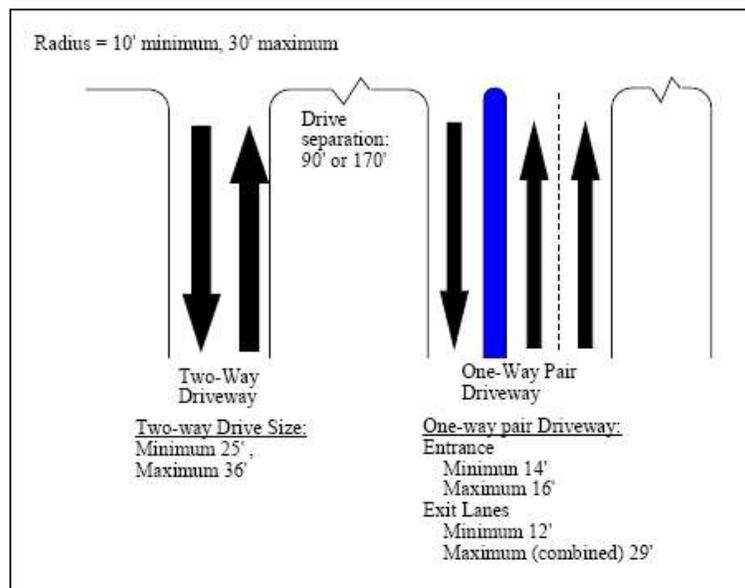
- (1) One-way driveways shall be not less than 12 feet nor more than 24 feet in width. A wider one way driveway may be required by the Sandy City Fire Marshall.
- (2) Exception: No two complementary one-way driveways may total more than 45 feet in width.

b. **Two Way.**

- (1) Two-way driveway approaches shall be not less than 25 feet nor more than 36 feet in width. In determining the width of curb openings and spacing of driveways, the end

transitions in each case will not be considered a part of the width of the curb opening.

- (2) Wherever a common driveway is constructed serving two or more properties, the common curb opening shall have a maximum width of 36 feet.
  - (3) The Planning Commission shall review and may approve or deny deviations to the above driveway width standards upon the recommendation of the Sandy City Transportation Engineer.
- c. **Fire Access.** If any portion of a building is more than 150 feet from a public right-of-way, the Sandy City Fire Marshall requires a minimum of a 20 foot wide driveway on all sides of the building for access.
  - d. **Drive Approaches.** All driveway approaches shall be equipped with curb radii and provide for handicap access.
  - e. **Minimum Curb Radius.** All drive approaches shall have a minimum end transition (curb radius) of 10 feet and a maximum of 30 feet.
  - f. **Driveways.** Driveways shall be located a minimum of 5 feet from the property line, measured from the throat of the driveway. This does not apply to property lines where a shared driveway is proposed.
  - g. **One-Way Pair Driveways.** Where a driveway is of the split, one-way pair directional type, there shall be a raised landscaped island of at least 5 feet in width between the two driveways.
  - h. **Maximum Curb Opening Coverage.** The total width of all curb openings shall not exceed forty (40) percent of the projects frontage. For corner lots, the total width of all curb openings shall not exceed thirty (30) percent of the combined frontages.
  - i. No curb opening will be approved which contemplates vehicle encroachment on any portion of the street right-of-way for loading, standing, or unloading.



- j. Curb openings must serve only those off-street parking spaces or loading zones that conform to Sandy City standards.

- k. Curb openings and driveways shall be paved and shall provide for adequate drainage.
- l. **Unused or Abandoned Drive Approaches.**
  - (1) Upon the issuance of a building permit, any unused or abandoned drive approaches or portions thereof shall be restored to the original curb section by the removal of the drive approach and replacement with high back curb and gutter to Sandy City Engineering Standards at the expense of the property owner adjoining that portion of the right-of-way.
  - (2) Upon refusal or neglect of the owner or agent to restore the unused or abandoned drive approach to their original high back curb and gutter section, the City shall proceed to do such work and all expenditures so incurred shall be charged against the owner or agent.
- m. Improvements in the public right-of-way shall be designed and constructed in conformance with the AASHTO specifications, including but not limited to the following:
  - (1) The minimum design vehicle shall be the single unit truck.
  - (2) No object shall be so situated as to interfere with the required sight-distance of intersections as set forth in the AASHTO specifications.
  - (3) All driveways shall intersect the street at a 90 degree angle to a distance at least 20 feet from the back of the property line.